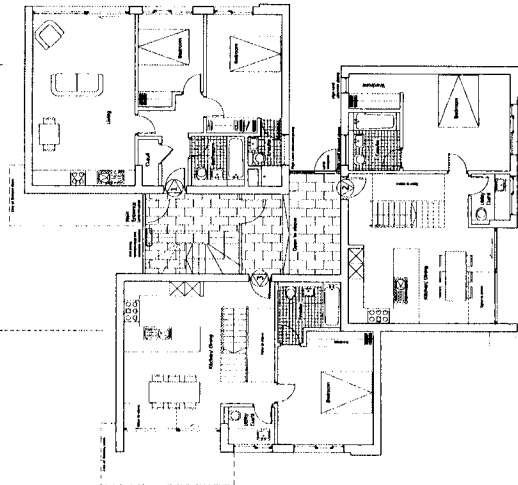
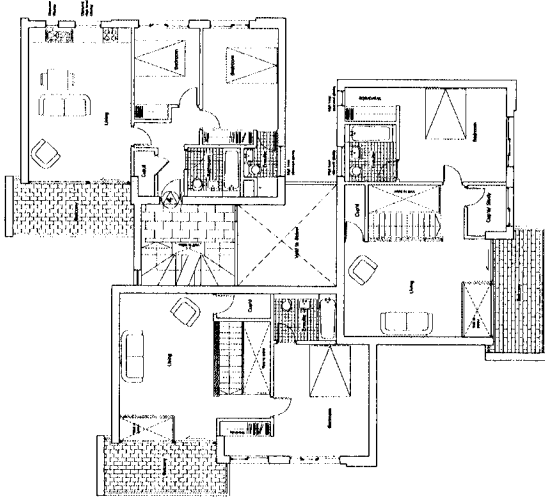


APPENDIX 1

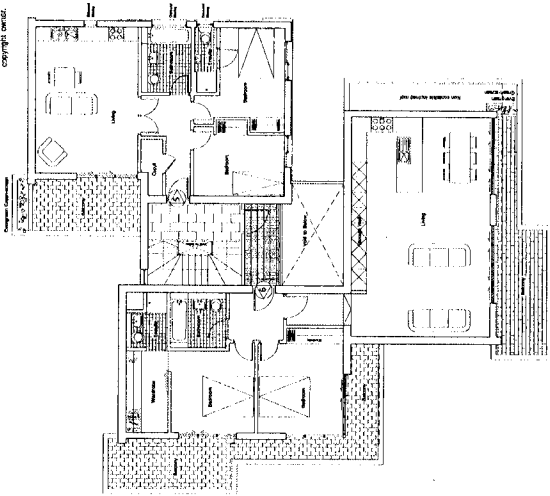
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Ground Floor



First Floor



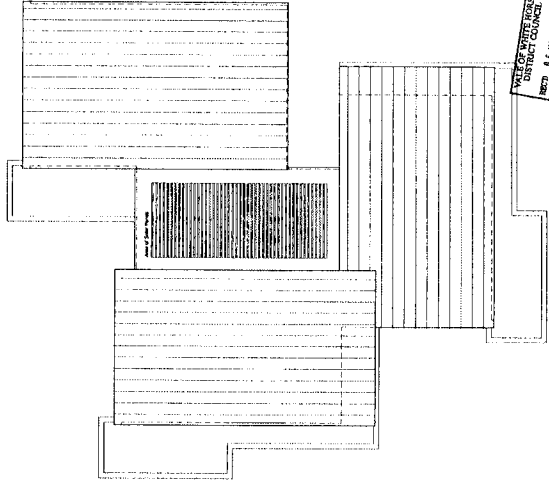
Second Floor

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1:1000 = 1:500
1:2000 = 1:1000
1:5000 = 1:2500

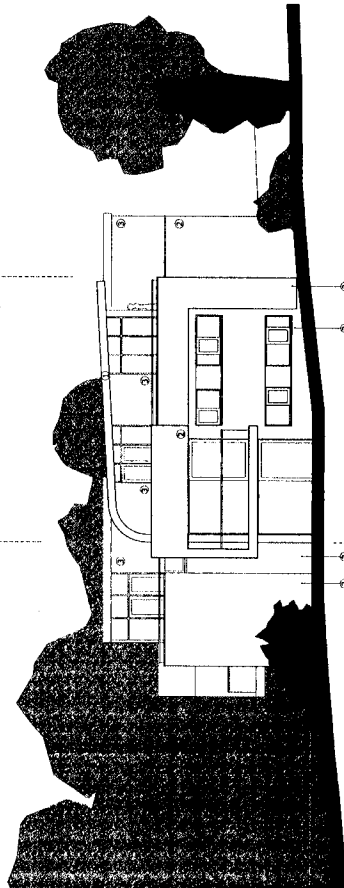
V07:
1. Standing Seam Zinc roof
2. 200mm concrete
3. Gable Cladding
4. 20% Cladding

RIACH ARCHITECTS
112-114
DUNEDIN CITY CENTRE
DUNEDIN 9012
Tel: 01865 553772 Fax: 01865 553775
email: ria@riacharchitects.com
www.riacharchitects.com

Client	Mr A.P. Smith
Project	81 Corners Hill Dunedin
Location	81 Corners Hill Dunedin
Project Name	PLANNING APPLICATION Proposed Floor Plans and Sections
Drawing No.	06-105-202
Scale	1:100 @ A1
Date	22.02.07
Revision	01/03/07 Full Compliance



Roof Plan



South Elevation

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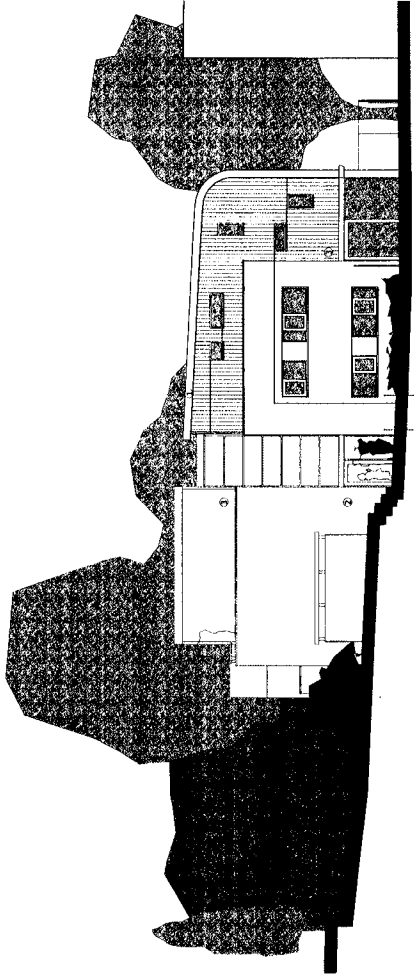
RIACHA
ARCHITECTS
INC.
OF PA
INCORPORATED
 Tel: 01985 5931
 email: riach@riach.com
 www.riach.com

CLIENT: Mr. [redacted]
 PROJECT: 61 [redacted]
 DRAWING TITLE: PLAN 1
 DRAWING NO.: 86-505-203
 DRAWING DATE: [redacted]
 REVISION: en 00341 JCA

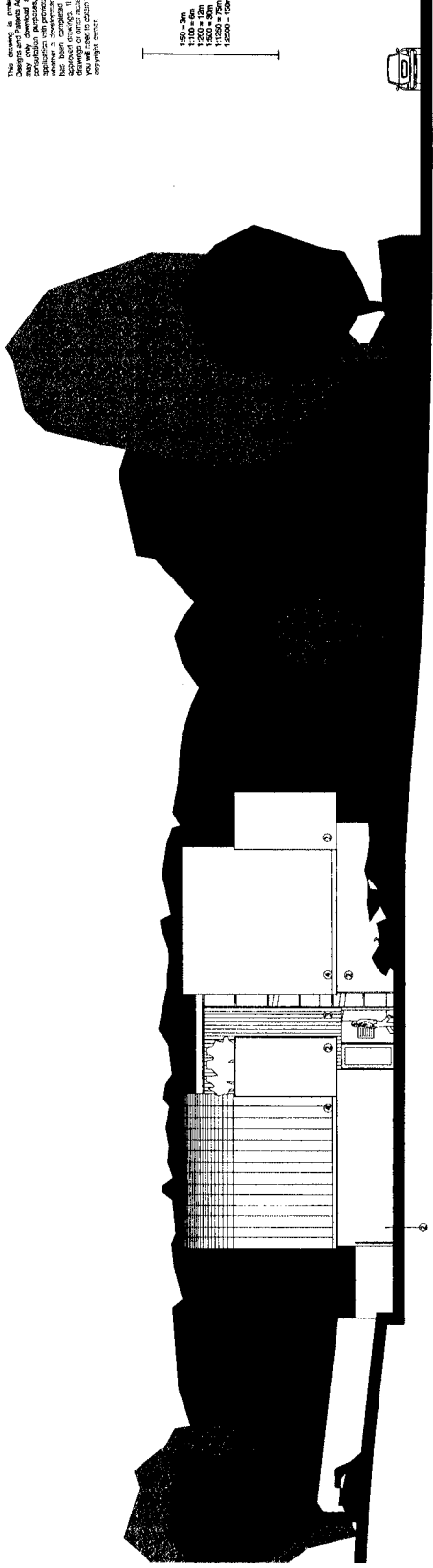
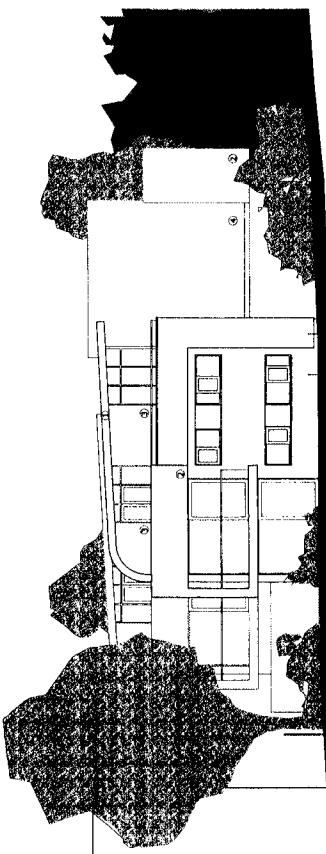
Scale: 1/4" = 1'-0"
 Date: [redacted]
 Drawn by: [redacted]
 Checked by: [redacted]
 Approved by: [redacted]

This document is prepared by the Contractor. Design and Planning are the Contractor's responsibility. The Contractor shall be responsible for all aspects of the project, including but not limited to, the design, construction, and maintenance of the project. The Contractor shall be responsible for all aspects of the project, including but not limited to, the design, construction, and maintenance of the project.

- KEY:
1. Standing Seam Zinc Roof
 2. Cedar Cladding
 3. Zinc Cladding



Cumnor Hill elevation (west)

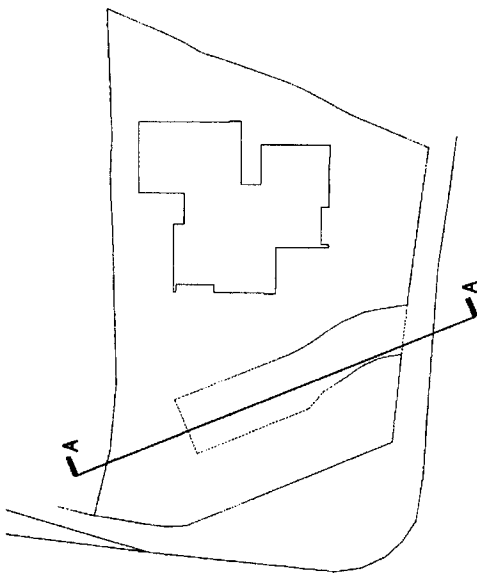


North Elevation (side)

1500 = 3m
 1200 = 2m
 1000 = 1m
 1500 = 3m
 1200 = 2m

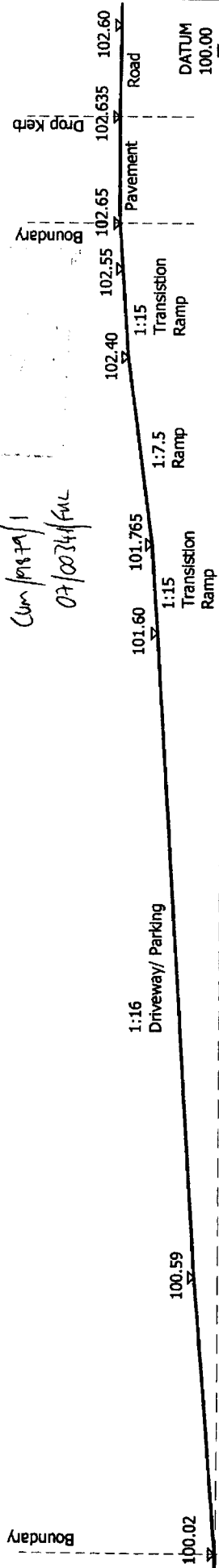
SCALE: 1/4" = 1'-0"
 DATE: [redacted]
 DRAWN BY: [redacted]
 CHECKED BY: [redacted]
 APPROVED BY: [redacted]

APPENDIX 1



SECTION INDICATOR

Walsley Valley District Council
 DISTRICT COUNCIL
 RECD 7 9 MAR 2007
 CORPORATE POSTAL
 SERVICES - 2



Cum 1/18/11
 07/0034/FUL

SECTION A-A

RIACH ARCHITECTS 65 BANBURY ROAD OXFORD OX2 8PE Tel: 01865 563772 Fax: 01865 563776 email: mail@riacharchitects.com www.riacharchitects.com	client Mr AR Smith	drawing title PLANNING APPLICATION Vehicular Access Ramp Section	scale 1:100 @ A3	date 28/03/2007	Do not scale - Use figured dimensions only All dimensions are approximate and should be checked on site prior to construction Discrepancies must be reported immediately before proceeding Copyright DHR - Drawings not to be reproduced or used without prior written permission
	project 61 Cumnor Hill Oxford	drawing no. 06-505-207	Revisions	rev date description	

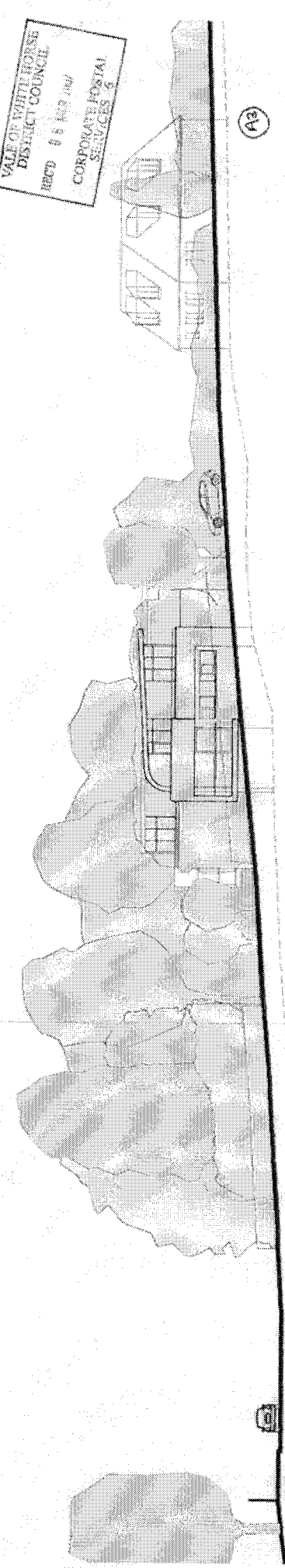
APPENDIX 1

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- 1:100 = 6m
- 1:200 = 12m
- 1:500 = 30m
- 1:1250 = 75m
- 1:2500 = 150m

VALE OF WHITE HORSE
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RIACH ARCHITECTS 66 BANBURY ROAD OXFORD, OX2 0PE Tel: 01865 553772 Fax: 01865 553775 email: riach@riacharchitects.com www.riacharchitects.com	
client	Mr A R Smith
project	61 Cornbar Hill Oxford
drawing title	Arnolds Way Street Elevation
drawing no.	scale
06-505-205	1:250 @ A3
drawing file	date
	22/02/2007
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ARNOLDS WAY

07 100341/511 Cum 19879/1

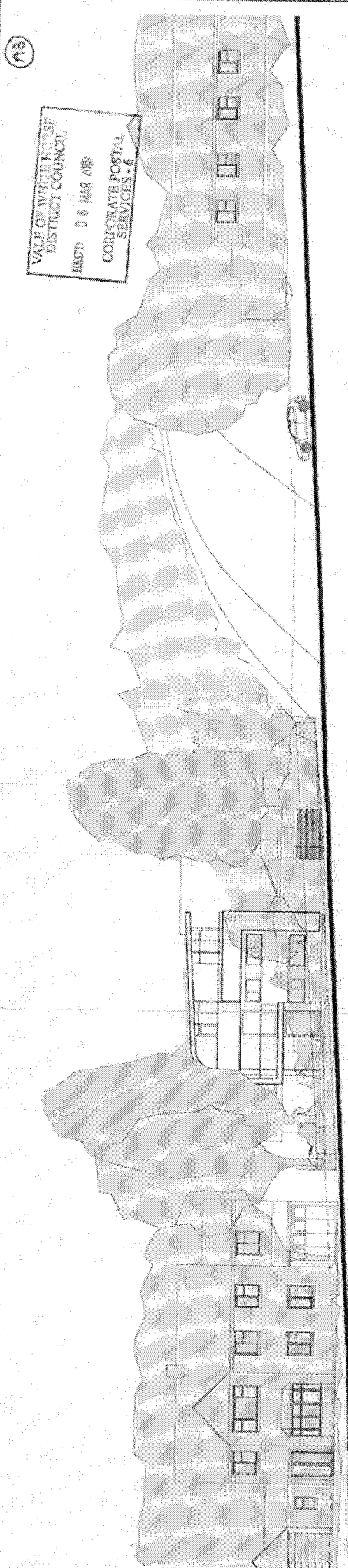
Neighbouring building elevations and ridge heights as measured from topographical survey. Elevations are indicative illustrations.

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 1:150 = 6m
 1:200 = 12m
 1:500 = 30m
 1:1250 = 75m
 1:2500 = 150m

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client	Mr A R Smith
project	81 Cumnor Hill Oxford
drawing title	Cumnor Hill Street Elevation
drawing no.	scale
08-506-204	1:250 @ A3
drawing file	date
	22-02-2007
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VALE OF WHITE HORSE DISTRICT COUNCIL
 REC'D 06 MAR 2007
 CORPORATE POSTAL SERVICES 6

CUMNOR HILL

07/00841/PL Cum/19874/1
 Neighbouring building eave and ridge heights as measured from topographical survey. Elevations are indicative illustrations.

A3

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- 1:200 = 12m
- 1:500 = 30m
- 1:1250 = 75m
- 1:2500 = 150m



(B3)

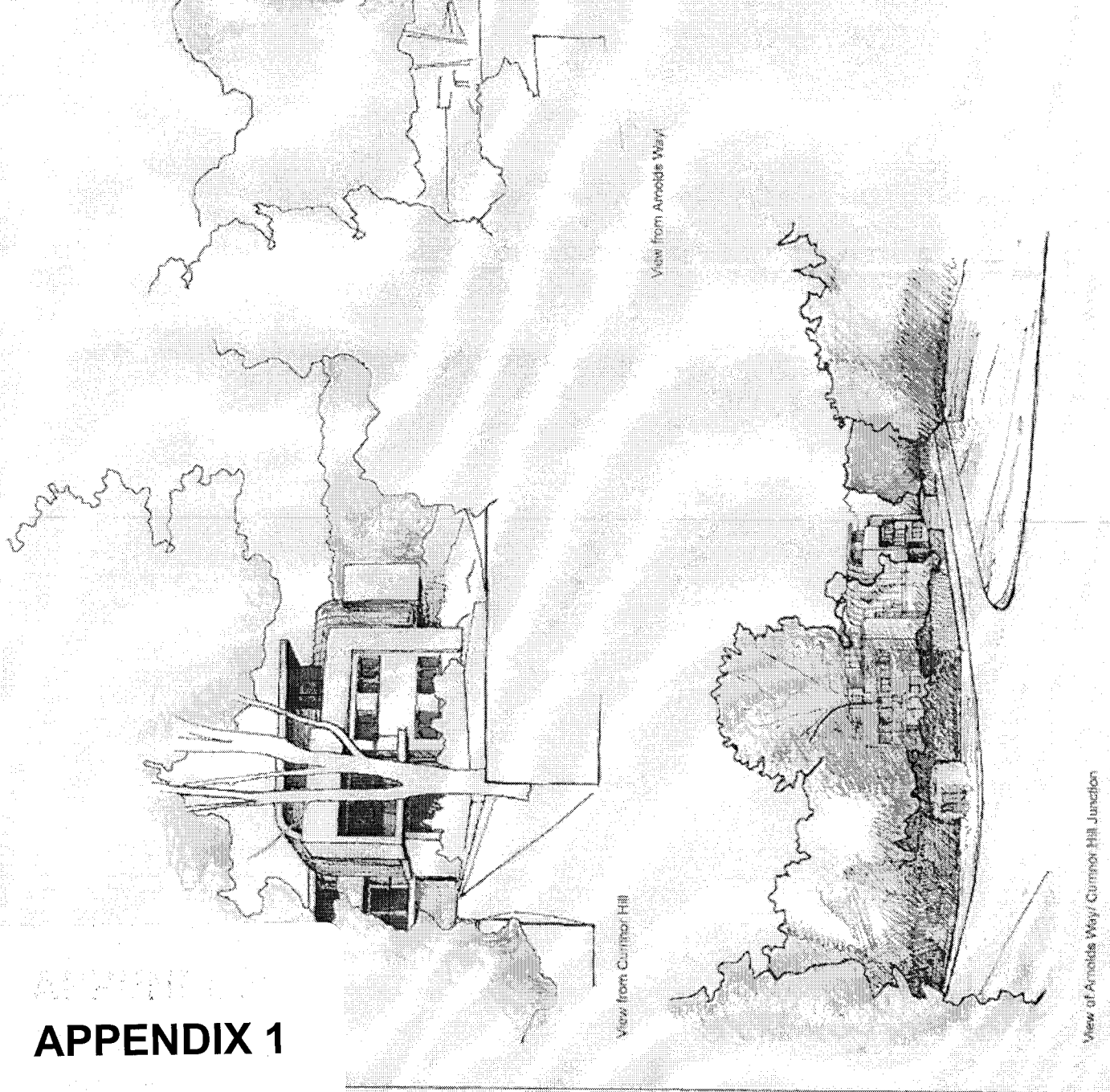
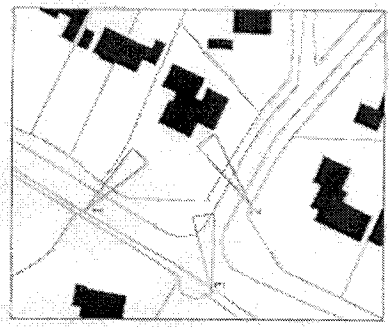
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RIACH ARCHITECTS
 85 BANBURY ROAD
 OXFORD OX2 8PE
 TEL: 01865 263772 Fax: 01865 583775
 email: mail@riacharchitects.com
 www.riacharchitects.com

client	M. A. R. Smith
project	61 Curmor Hill Oxford
drawing title	Illustrative rendering sheets
drawing No.	06-009-006
scale	NTS
drawing file	file
date	22/07/2007

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View from Curmor Hill

View from Arnolds Way

View of Arnolds Way/ Curmor Hill Junction

This Design Statement is offered in support of the planning application for the redevelopment of 61 Cumnor Hill, Oxford. The intention of this document is to describe and illustrate the proposal within the context of the design and planning process. The application proposal is the development of considerable pre-planning consultation with the Local Planning Authority

APPENDIX 1

Site History

The site and existing building are in use as a single residential dwelling. The 1930's house has several more recent additions, including a two storey extension to the side and integrated garage with roof terrace above. There looks to be no relevant planning history connected with this site.

It appears, in the historic creation of no.3 Arnolds Way, (which stands immediately to the rear of no.61), that it was formed through the division of no.61 rear garden, preceding the implementation of the planning system as we know it.

Site Description

The site is approximately 1300sq.m and forms a corner plot with a 30m frontage onto Cumnor Hill and 40m frontage to Arnolds Way. Pedestrian and vehicular access is via a gateway onto Cumnor Hill. A single driveway leads up to the garage and main entrance of the house.

The site has a verdant and suburban appearance. This is reinforced by the virtue that all buildings are set back from the carriageways and remote from their site boundaries.

The area has a good provision of public transport, serviced by a series of bus stops adjacent to the site with one directly at its entrance. Frequent bus services include that between Abingdon and Oxford City with more occasional routes to Wantage, Grove, Faringdon and Swindon. For cyclists, upon reaching the base of Cumnor Hill, the dedicated network of Oxford cycle ways and paths allows good access to the city centre.

Site Context

The site is located on the periphery of the built up area of Botley, one of the five main settlements within the Vale. Its proximity to Botley local centre and Oxford City beyond make this urban fringe site suitable for sustainable living.

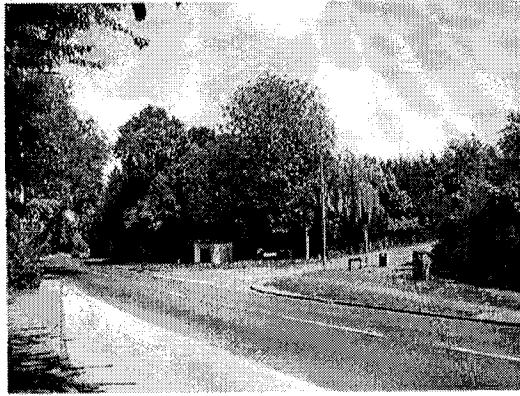
The demographic could be described as:
'many of the people who live in this sort of postcode will be wealthy mature professionals living in large houses. This type of postcode encompasses the most affluent people in the UK. They live in wealthy, high status suburban and semi-rural neighbourhoods. Most are highly qualified professionals, senior executives and business owners, often in their 40s and 50s. They tend to live in large detached houses with four or more bedrooms, many of which are owned outright. These households often have more than two cars.'

(source: upmystreet.com)

There is a mix of housing styles and design, dependant on age of construction. Cumnor Hill itself is made up of mainly 1930's large detached dwellings, isolated on large sites, with some modern development and infilling. Arnolds Way leads to a more recent suburban estate as well as school and other local amenities.



Location Plan (as proposed)



South-West view of no.61 along Cumnor Hill

The Proposal

The proposal is to replace the existing house and redevelop the site to provide six 2-bedroom apartments over 3 storeys, each with private amenity space, car parking provision, covered cycle parking and enclosed bin storage.

The area of parking is created to form a buffer zone between the private/ living part of the site from the public/ more discordant roadway.

The siting of the proposed building is in accordance with existing framework, that is, away from the site boundaries. Existing vegetation to the boundaries is to be retained and further planting proposed in order to maintain the suburban character of the area.

Design Principles

Respecting the existing verdant character and providing a sensible and respectful approach to the site is the guiding design principle for the proposal. It is also an objective to provide high quality spaces in which to live, as well as a responsible and integrated design, which will enhance its surroundings.

From early inception, together with planning advice and support, a key design decision was to create a building of contemporary nature. Whilst the site is not devoid from context, the mature trees and screening that is already in place suggested that a building could be created that is contemporary while still being sympathetic, with out fear of replication and pastiche of historic styles.

The three-storey modern design is offered as an alternative to creating a more traditional three-storey building with accommodation in the roof space. The line of the top floor is proposed as set back from the main building elevation. The effect, in the same manner of a pitched roof, reduces the visual impact of this crowning element.

A fragmented building mass is proposed, comparable to that of a large suburban house. Three small blocks are linked by one circulation/ service core. Each block is orientated with specific aspects, so as to have regard for each of the apartments and neighbouring properties privacy. The apartments are designed to provide modern flexible living accommodation, with good natural daylighting and their own private balcony, again which avoid unnecessary overlooking.

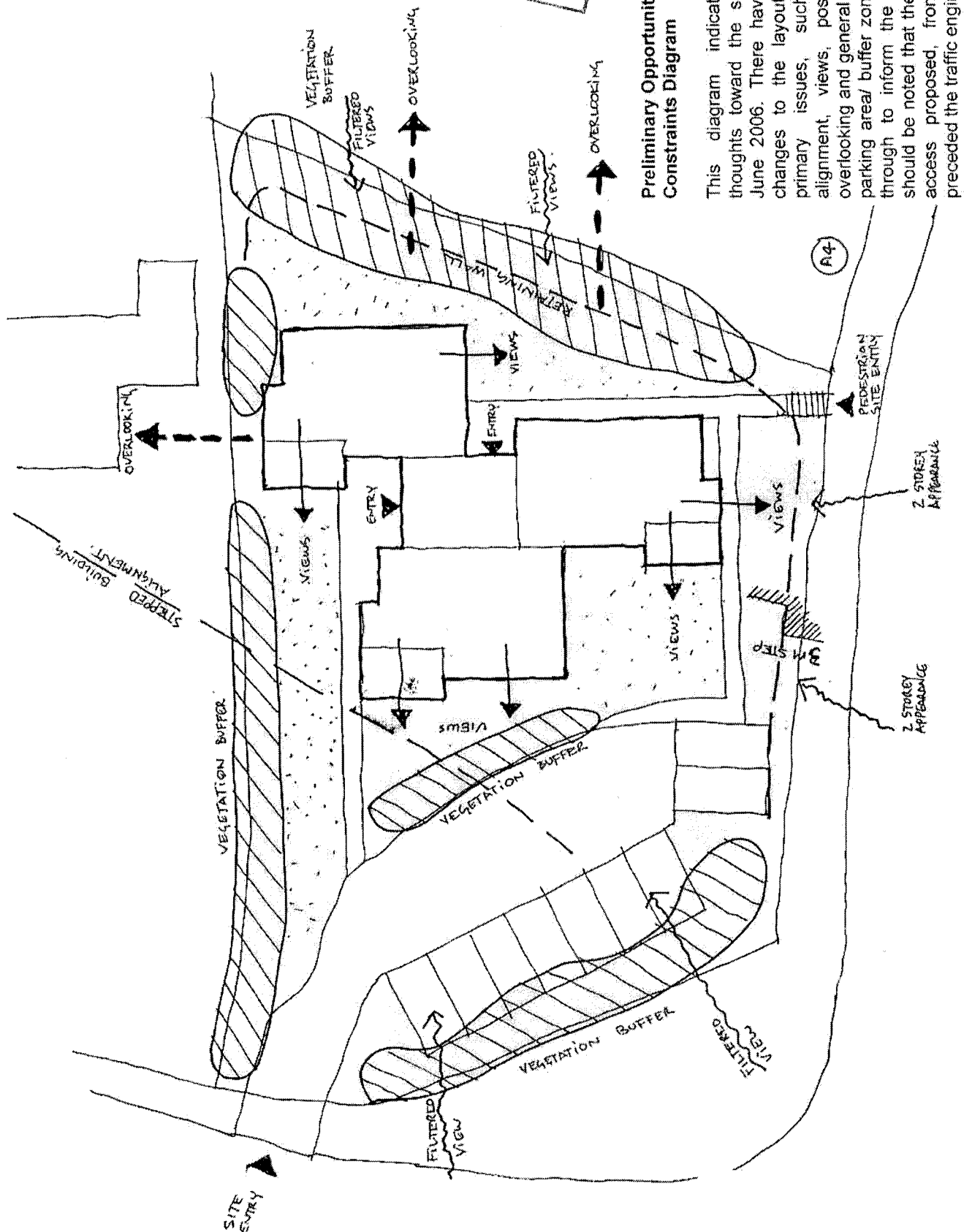
View along Cumnor Hill to the South



The existing site slopes up slightly from front to rear, whilst the road levels along Arnolds Way increase quite significantly, in the order of an increase of 4metres. This change in level has been incorporated into the design, the proposed three-storey building will therefore, if it were possible to view through the evergreen vegetation, offer a 2-storey building at street level.

The landscape is utilised for internal recreational use. It is an important aspect of the design that each apartment enjoys its own exclusive outdoor living space. The area for car parking has been designed to minimise its impact and so as not to present a monolithic slab of hardstanding.

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Preliminary Opportunities + Constraints Diagram

This diagram indicates our initial thoughts toward the site layout, from June 2006. There have been various changes to the layout, however, the primary issues, such as building alignment, views, possible areas of overlooking and general layout, including parking area/ buffer zone have followed through to inform the final design. It should be noted that the initial vehicular access proposed, from Cumnor Hill, preceded the traffic engineer's advice.



View along Arnolds Way towards Cumnor Hill

Design Principles cont...

Parking bays are conceived as part of the landscape are designed to be part of the garden whether in use or not.

The existing boundary trees are considered to form an important part of the site and character of the area and are to be protected. The 'no-dig' method for constructing parking near to trees is specified in accordance with Arboricultural Practice Note (APN) 1 – Driveways close to trees. Refer to appendix for more details.

It is proposed to use good quality materials. Briefly, external walls will be faced with crisp render. A Zinc standing seam roof is proposed atop the second floor. This roofing element, at a slight pitch of 3°, curves and wraps around to form part of the external wall element. This type of roof covering is the same crafted roof as applied to the new buildings at Radley College, also within the Vale. The fine seam lines give a slim appearance and, as demonstrated, harmonise with both traditional and modern architecture, this is key to the integration of the contemporary design.

The hybridising, or mixing of the materials will provide an accurate account of the street character, resulting in a response that is contemporary whilst still well-balanced with its surrounding context.

This development seeks to respect its neighbours. Concerns of overlooking have been addressed by orientating upper level main living spaces away from neighbouring properties. Where light would benefit a room, but overlooking could have been considered an issue, high level or obscured windows have been specified. Balconies are also screened where necessary with 'green screens' which provide evergreen cover on a modular trellising system. Refer to appendix for further details.

Accessibility

The existing site is located on Cumnor Hill and therefore possesses steep gradients to the site and locality.

In designing a new proposal to this site, it has been an aim to develop a coherent access strategy that offers the best possible solution given the site constraints.

The main pedestrian and cycle access is proposed as retained from Cumnor Hill. This conveniently links to the existing bus stops and roadway for cycle use to and from Oxford. The shallow gradient affords easy access for all users.

A previous access to the rear is also re-established as a secondary pedestrian access. This inclined access, designed to 'inclusive design' standards and in co-ordination with the Vale's Waste Management department, includes for the refuse/recycling store. Refuse is intended for collection from Arnold's Way.

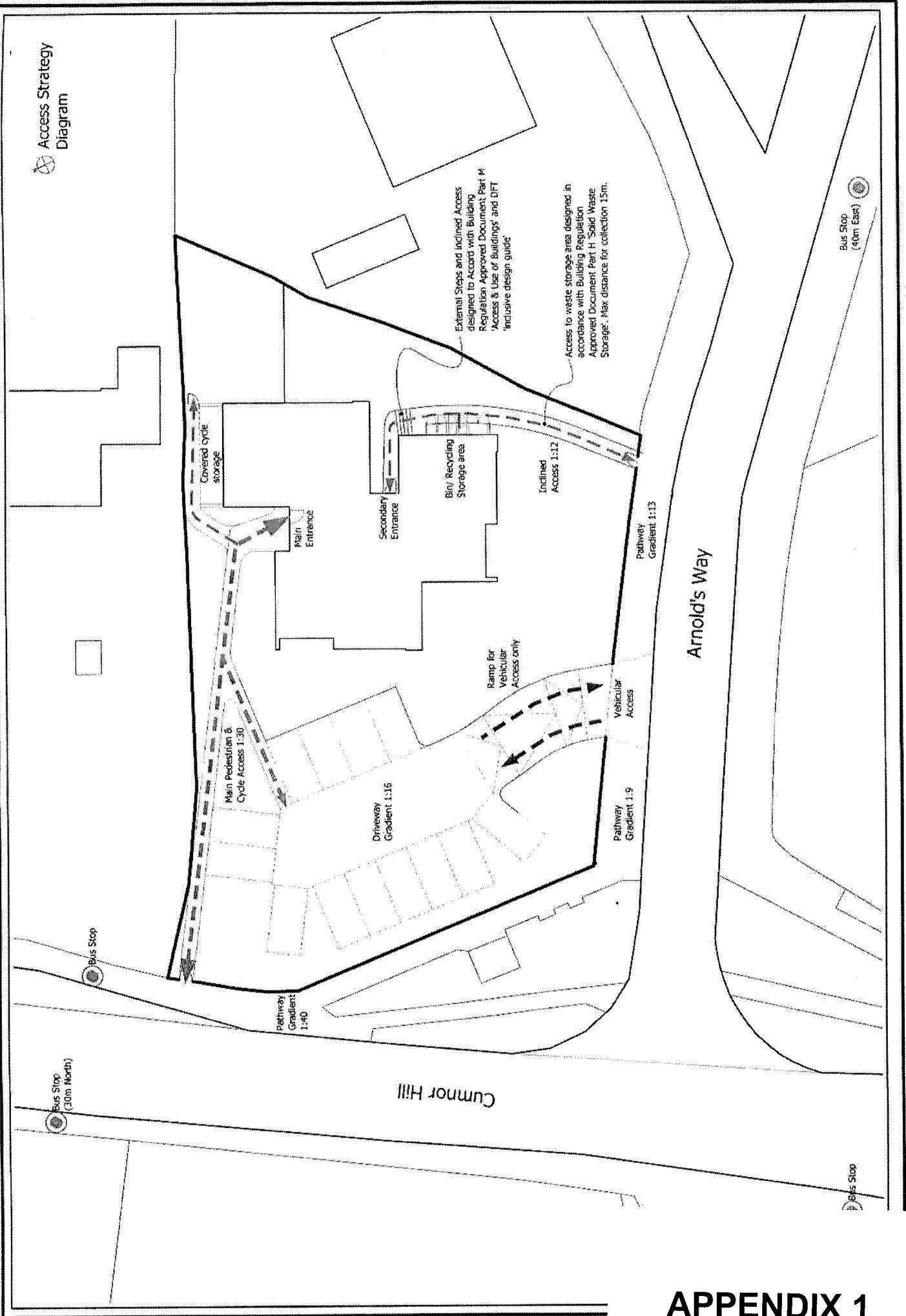
The existing vehicular access is proposed as closed off and a new access created onto Arnold's way. This decision was a result of the transport assessment, also forming part of this application. This ramped driveway is designed solely for vehicular use. The main Cumnor Hill access providing a more suitable access for all.

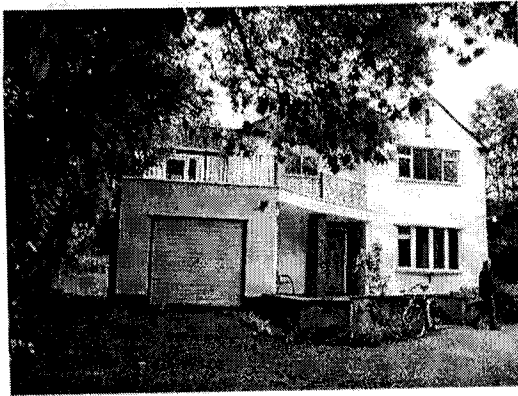
APPENDIX 1

South-east view of no.61's boundary along Arnolds Way



Access Strategy
Diagram





View of existing dwelling

APPENDIX 1

Planning Policy

In the preparation of this planning application, the Vale of White Horse Local Plan and Government planning guidance have been given full regard. Particularly relevant to this proposal are:

H1_ This development will help to meet housing target highlighted by the structure plan requirement to 2011 as identified by chapter 8.11 of the local plan. This development satisfies and goes towards the 370 dwellings estimated to result from development on sites which cannot yet be identified by the local plan.

H9_ Located within the built-up area of Botley, as identified by the development boundaries on the proposals map, the proposal makes efficient use of land and is designed so that the layout, mass and design of the dwellings will not harm the character of the area.

H15_ seeks to provide a variety of dwelling sizes, tenures and types that will meet the needs of existing and future residents. The number of one and two person households is expected to increase in the future and there is a need to increase the number of small dwellings available. This is especially important in an area of high house prices like the Vale, where people may not be able to afford the home of their aspirations. The Council's *Housing Needs Survey* shows that there will be a shortage of one and two bedroom dwellings in all tenure groups in the coming years. The consultants who prepared the survey considered that the shortage was so acute that the Council should be seeking 97% of the new dwellings in the owner occupied sector to have one or two bedrooms.

H23_ Providing safe, attractive and convenient outdoor space is key to each dwelling created. To this end, each dwelling enjoys its own private amenity space in the form of a private enclosed garden or generous balcony. The remaining garden space will be landscaped for all residents enjoyment as communal amenity space.

Sustainable Design Strategy

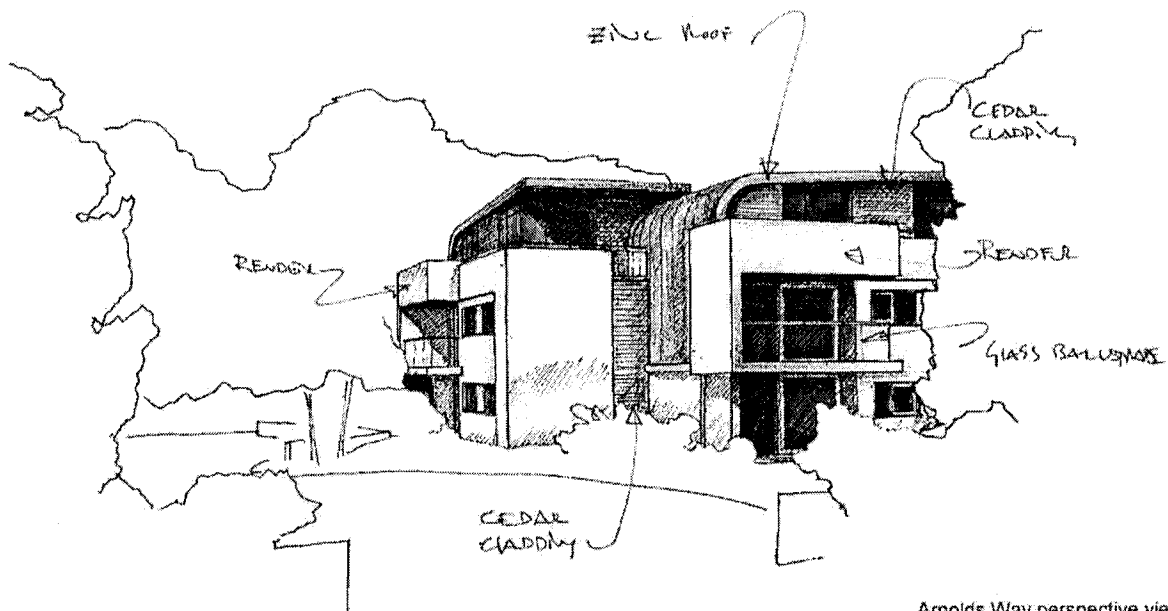
In all aspects of the proposal sound design principles in accordance with sustainable and green building policies will be adopted. The following are key sustainability objectives for the project.

- Making efficient use of this previously developed land.
- Provide amenities that promote a good healthy lifestyle including bicycle parking, communal space, natural light, and optimal orientation.
- Where possible use of recycled/re-used/reclaimed or renewable materials.
- A Zinc roof is proposed as it is naturally resistant to corrosion, is extremely durable (40-100 years) and does not require costly maintenance. Zinc is 100% recyclable. More than 90% of the zinc used in the building industry is recycled, putting it far ahead of many other materials. Less energy is required to produce zinc than the other principal metals.
- Where possible use of new materials that have low VOC emissions

View of garden space



- Sealing from the outside, preventing further heat loss.
- Specification of energy efficient and water saving devices and appliances.
- The harvesting of rainwater for reuse, thereby reducing the water consumption and minimising flood risk through attenuation.
- Initiate sustainable construction practice with suitable contractors including, recycling and waste management.
- Photovoltaic cells to the flat area of roof, connected to the grid, will provide pollution and noise-free electricity for the landlords supply, for example, internal and external security and lighting. During the day, the electricity generated by the PV system can either be used immediately or can be sold to one of the electricity supply companies. In the evening, when the solar system is unable to provide the electricity required, power will be bought back from the network. Therefore in effect, the grid is acting as an energy storage system. The key objective is to achieve an energy neutral supply.
- The use of a local workforce during construction, thereby in support of the local economy.
- By providing more housing and densifying this will help to sustain local services and public transport, conserve natural resources and minimise the amount of Greenfield and other open land being taken for development.



Arnolds Way perspective view and proposed use of materials

Summary Statement

We believe our proposal fully conforms to the local plan and national government policy guidance, both current and emerging, with particular regard to the re-use of previously developed land. This proposal represents a sensible, well-balanced use of the site. Considering its location and the potential of the proposal, the project represents an ideal solution supporting current development policies of the Vale of White Horse District Council.

CUMNOR PARISH COUNCIL RESPONSE

The observations of Cumnor Parish Council.

Computer No. 07/00341/FUL Officer: Mr Stuart Walker

Application Number: CUM/19879/1 Amended plans: Yes

Address of Proposal: 61 Cumnor Hill, Oxford, Oxon, OX2 9EY

Proposal: *Demolition of existing dwelling. Erection of six 2 bedroom apartments with car parking, covered cycle store and refuse/recycling store.*

Please select the response that most accurately reflects your views on this application by ticking one box and providing the relevant reasons where this is requested, using a separate sheet if required.

1. Fully support for the following reasons:

2. No objections.

3. Do not object but request the following issues be given consideration:

4. Object for the following reasons:

There appears to have been little changed with this application except for the use of some different materials, the car parking has increased from 9 spaces to 12 spaces and some rooms in the third storey have been moved around. The developer has not addressed any of the other matters that caused widespread objections.

Character and Design. The proposed development is out of keeping with the character of the surrounding residential area, which is made up of mainly single large houses set in extensive, mature and landscaped gardens. The developer has made no attempt to design this to look like a large house, but it will be seen quite plainly as a block of flats more in keeping with a seaside resort. The layout, mass and design of the building would harm the character of the area.

The footprint of the building would be three times that of either the neighbouring properties No 59 Cumnor Hill and 3 Arnolds Way and of the existing house.

The height of the proposed block of flats would be higher than the existing house and would give the appearance of overdominance.

The building would be highly visible with little screening, and in terms of scale, density (46.15 dwellings per hectare), layout and access would not complement the neighbouring houses or the street scene.

Access and Road safety. The Council is very concerned about the access to the proposed new development, which will be up a slope and onto Arnolds Way, and only 25m from the very busy junction with Cumnor Hill. Any overspill of parked cars onto Arnolds Way would be hazardous, particularly as the road is often congested with cars and buses.

The Council recommends that the Highways authority carries out a comprehensive survey at peak times to properly ascertain traffic movements and the feasibility of the proposed access.

Neighbourliness. The Council believes that the design, size and positioning of the flats with their large obtrusive balconies would unacceptably harm the amenities of neighbouring properties, particularly to No 59 Cumnor Hill and 3 Arnold's Way, in terms of a loss of privacy, daylight/sunlight, dominance and intrusion.

Car parking and Amenity space. Although the number of car parking spaces has been increased from 9 to 12, this is at the expense of the usable amenity space, which is likely to be confined to a thin triangular strip of land squeezed between the western side of the building (9m high) and existing trees on the border with 3 Arnolds Way, and will be in semi-shade all the time.

Sewage. The Council states again its concerns about the impact on some areas on Cumnor Hill and in Dean Court because of the number of new dwellings being built on and near to Cumnor Hill and the apparent inadequacy of the drainage and sewage system in the area to cope with additional sewage and run-off of surface water. This is likely to have a cumulative effect on areas in the Dean Court ward and puts properties at a very real risk of flash flooding.

Sustainability. The site is some way up Cumnor Hill and occupants of the new flats are unlikely to walk or cycle to the shopping area in Botley.

The Council notes that this type of piecemeal application does not provide affordable housing and doubts if the area can accommodate any more 1 and 2 bedroom apartments, and if there is sufficient demand for this type of accommodation.

It is strongly recommended that the views of the local residents are taken into consideration.

Signed by ...*J B Bock*.....

Dated 3 April 2007

Clerk to Cumnor Parish Council

McCoy Associates Chartered Town Planners

54 New Street □ Henley on Thames □ Oxon RG9 2BT □ Tel: 01491 579113
Fax: 01491 410852 www.mccoyassociates.co.uk email denis@mccoyassoc.co.uk

4 April 2007

your ref CUM/19879/1

For the attention of Alison Blyth
Deputy Director (Planning and Community Strategy)
The Vale of White Horse District Council
PO Box 127
The Abbey House
ABINGDON OX14 3JN

email and post

Dear Sir

**re: Demolition of existing buildings. Erection of six 2 bedroom apartments with car parking, covered cycle store and refuse/recycling store
61 Cumnor Hill, Oxford**

Thank you for the drawings of this project received on 29 March which was discussed at the Architects Panel meeting on 4 April and on which you have requested design comments.

This is a challenging contemporary design which has attracted a good deal of adverse local comment. My own view however is that the favourable appraisal in my letter of 12 January 2007 is fully justified by the more comprehensively presented details now available, certainly so far as the proposed building is concerned.

I still regard its design as one which is capable of enhancing the character and quality of the area. Diversity of design is a characteristic of this verdant suburb, and it contains some buildings which in their time would have been regarded as novel departures from the usual.

This application can be so described, but I believe displays a design integrity which merits Members' support and is likely eventually to be admired.

The importance of trees and shrubs on and around the site is clearly demonstrated by the sketches, so a condition to deal with the protection and enhancement of the landscaping on site will be vital.

My one concern is that the number of parking spaces which have to be provided, and the width of the entrance and access to them, detract significantly from the merits of the proposal.

Your papers and drawings are returned with the postal copy of this letter.

Yours faithfully

McCOY ASSOCIATES

encs

cc Stuart Walker by email

This letter refers to drawings nos 06-505-201 to -206 and (from website) design and access statement.

Denis F McCoy DiplArch(Oxford) ARIBA FRTPI FRIAI

Christopher R Baker Company Secretary

McCoy Associates Limited, company registered in England no 4457
VAT No. 363 3525 59

APPENDIX 3